



INSTRUCTIONS

Ultra-Gold™ ARC Shaft-Mount Aluminum Rocker System For Ford FE Engines (Part # 19046)

Thank you for choosing COMP Cams® products; we are proud to be your manufacturer of choice. Please read this instruction sheet carefully before beginning installation, and also take a moment to review the included limited warranty information. Contact us toll free at 1.800.999.0853 or at www.compcams.com under Tech Support with any questions.

Note: COMP Cams® strongly recommends use of new COMP Cams® pushrods whenever rocker arms are being replaced. This ensures that mating surfaces are free of any wear that could result in premature failure of the components. If old pushrods are being used, inspect them for any signs of wear, especially at the rod ends. Any wear on these surfaces will result in product failure.



Note: Each kit comes with two of the rocker assemblies shown in Fig. 1.

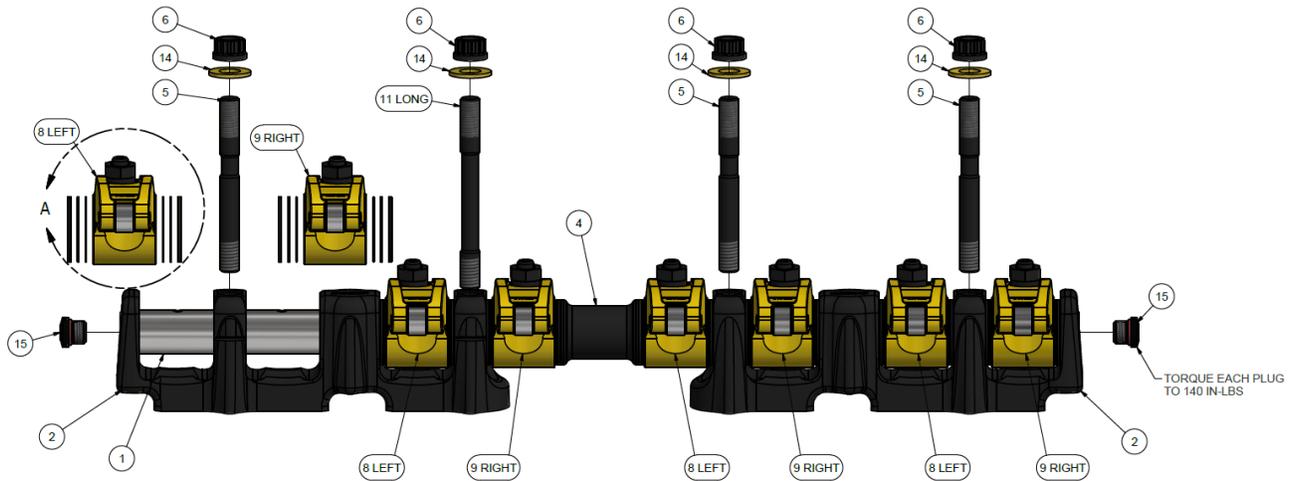


Fig. 1 - Rocker Assembly

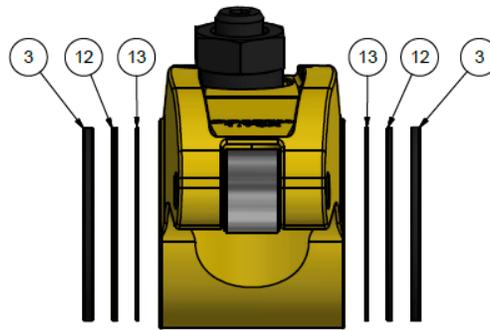


Fig. 2 - Shim Assembly

BILL OF MATERIALS			
ITEM	QTY	PART NUMBER	DESCRIPTION
1	2	19047P	FE ROCKER SHAFT
2	4	19046MCAS	FORD FE ROCKER ARM STAND
8	8	19046L	ULTRA GOLD FORD FE ROCKER ARM - LEFT
9	8	19046R	ULTRA GOLD FORD FE ROCKER ARM - RIGHT
5	6	19046STUD	FORD FE ROCKER STAND RETAINING STUD
11	2	19046STUD-B	FORD FE ROCKER STAND RETAINING STUD - OILING
14	8	301180WB	3/8 FLAT WASHER
6	8	19046NUT	3/8-24 12 POINT NUT
4	2	19046RS	SPACER
3	32	19046RW060	.060 SHIM
12	32	19046RW030	.030 SHIM
13	32	19046RW015	.015 SHIM
15	4	19046P	-4AN SOCKET HEX PORT PLUG

Table 1 - 19046 Kit Components

Installation Instructions

1. Remove the valve covers from the engine to provide access to the rocker arms.
2. Remove the existing rocker arms, studs, and pushrods from the engine.
3. Separate the 1/2" 12-point nut from each of the provided studs. (**Note:** One of the four studs is longer than the others and has a larger oil groove. This stud will always go in the oiling stud location second to the leftmost position, as depicted in **Fig. 4**. If the incorrect stud is put in this location, the rocker arm assembly will not receive the oil required for lubrication.)

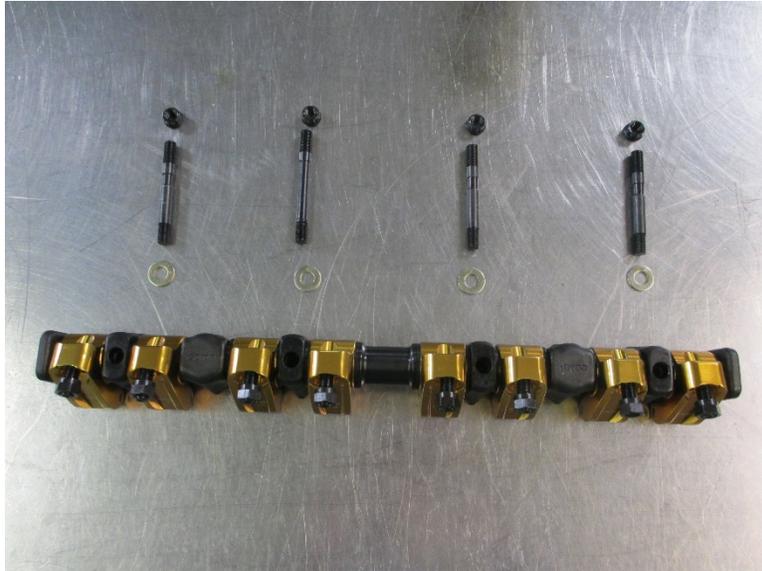


Fig. 3 - Disassembled Rocker Studs

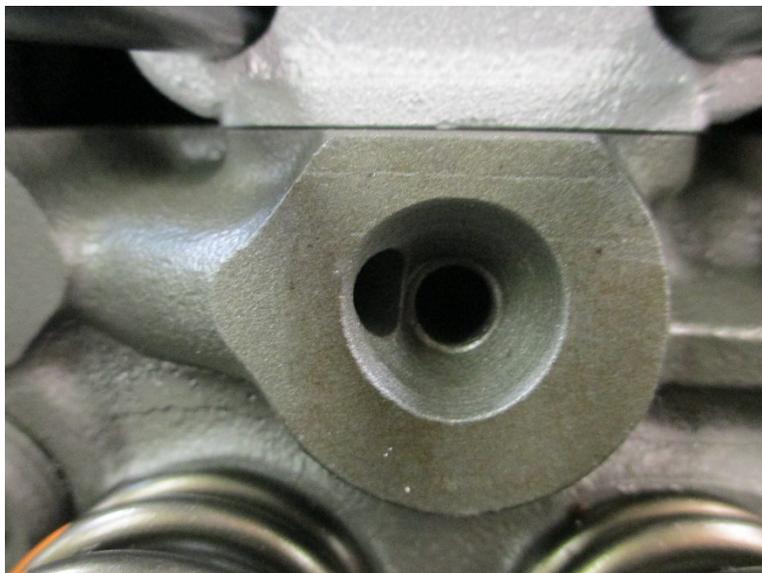


Fig. 4 - Oiling Stud Hole

4. Using a 1/8" hex key, back the pushrod adjusting screws out as far as they will go without tightening.
5. Be sure that the rocker arms are in the correct left-right order (refer to **Fig. 1**), and that the spacing of the rockers is appropriate for your application. If the incorrect left-right order is used, the roller tip will not be centered on the valve. The arrangement of the rockers can be adjusted by simply sliding the center shaft out of the assembly and rearranging them as needed.

Note: Each rocker arm comes with three spacers (0.015", 0.030", and 0.060") on either side of the rocker. These spacers can be rearranged to change the spacing of the rockers as needed. See **Fig. 2** and **Fig. 5**.



Fig. 5 - Rocker Assembly with Rocker Removed and Spacers Shown

6. Hand tighten the studs into their respective holes.
7. With the pushrods in place, slide the full assembly onto the studs. Rotate the rocker tips up, and pull the pushrods away from the cylinder head to allow the assembly to slide all the way onto the studs. (**Note:** Be sure that all pushrods are properly seated in the lifter seats. Failure to do so could result in coil binding the springs and bending pushrods.)

Note: The 19046 rocker system is set up to utilize 3/8" non-oiling cup style pushrods (such as COMP P/N 7999-16). If a different adjuster is desired, a 3/8-24 thread will be required.

8. Because the rocker assembly must be installed with the rest of the valve train in place, one or more of the pushrods will be lifted by the cam. This will prevent the rocker assembly from making contact with the head before tightening the stud nuts. In order to prevent product damage, use a 1/2" 12-point socket to tighten the stud nuts until the gap between the rocker assembly and the block is uniform. Then tighten each nut 1/2 turn at a time in a 1-3-2-4 order until the rocker assembly firmly contacts the block.

Note: If aftermarket head bolts are used, clearancing of the bolt head may be required in order to prevent the head bolt from contacting the rocker stands. See **Fig. 6** below.

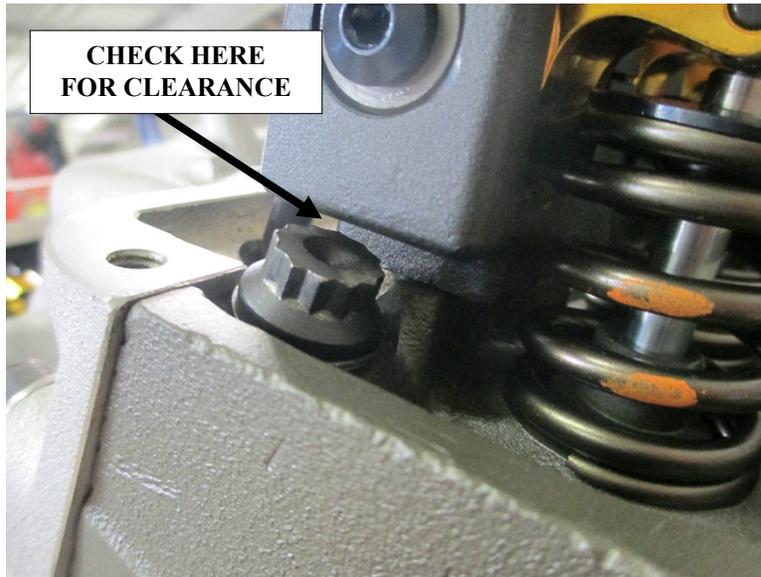


Fig. 6 - Bolt Head Clearance

9. After the assembly is firmly against the block, use a torque wrench to tighten each nut to 30 ft-lbs in the same 1-3-2-4 order.
10. Set the lash or preload on each rocker using a 1/8" hex key and a 9/16" wrench. Refer to your camshaft or lifter specifications for lash or preload.



Fig. 7 - Finished Rocker Assembly

11. Reinstall the valve covers on the engine.

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Competition Cams, Inc. warrants that all of its products are free from defects in material and workmanship, and against excessive wear for a period of (1) one year from the date of purchase. This **limited warranty** shall cover the original purchaser.

Competition Cams, Inc.'s obligation under this warranty is limited to the repair or replacement of its product. To make a warranty claim, the part must be returned within (1) one year of purchase to the address listed below, freight prepaid. Items covered under warranty will be returned to you freight collect.

It is the responsibility of the installer to ensure that all of the components are correct before installation. We assume no liability for any errors made in tolerances, component selection, or installation.

There is absolutely no warranty on the following:

- A) Any parts used in racing applications;**
- B) Any product that has been physically altered, improperly installed or maintained;**
- C) Any product used in improper applications, abused, or not used in conjunction with the proper parts.**

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